



## ARLINGTON COUNTY, VIRGINIA

**County Board Agenda Item  
Meeting of December 12, 2009**

**DATE:** December 4, 2009

**SUBJECT:** SP # 389 SITE PLAN AMENDMENT to modify Conditions #1, 19, 51, 64, 68, 77, 78, and 79, reduce amount of retail by approximately 1,845 sq ft, increase by 13 units number of residential units, and façade modifications, located at 2020 Wilson Blvd. and 2021 Clarendon Blvd. (RPC #17-011-009, -013, -015, -016).

**Applicant:**

Skylark Investments, LLC

**By:**

Martin D. Walsh, Attorney/Agent  
Walsh, Colucci, Lubeley, Emrich & Walsh  
2200 Clarendon Blvd., 13<sup>th</sup> Floor  
Arlington, Virginia 22201

**C. M. RECOMMENDATION:**

Approve the site plan amendment request subject to all previously approved conditions and modified conditions #1, 19, 51, 64, 68, 77, 78, and 79, of approved SP #389, to reduce the amount of retail by approximately 1,845 s.f. to 32,840 s.f., to increase the number of residential units to 154 units, and to modify the building's façade.

**ISSUES:** The proposed amendment would reduce the amount of retail space and increase the number off residential units and add balconies to some units thereby changing the building's exterior façade. No changes are proposed to either the approved building's height or tapering. There are no issues.

**SUMMARY:** SP #389 was approved for 141 residential units and 34,685 s.f. commercial/retail space in February 2007. The applicant is requesting to reduce the amount of retail space by approximately 1,845 s.f., to increase the number of residential units by 13, and to add balconies to some residential units. The applicant has agreed to support bike sharing in the Courthouse area and will submit to the County a \$12,000 contribution to go toward County bike sharing initiatives (Condition #51). Staff supports the proposed amendment. The changes result in an improved site plan that is consistent with the County Board's approval of the 2007 plan.

County Manager: BMD/GA

Staff: Richard Dooley, CPHD Planning  
Robert Gibson, DES, Division of Transportation

6.

PLA-5396

**BACKGROUND:** A mixed-use building containing 141 residential units and 34,685 square feet of commercial/retail space was approved on February 26, 2007. The proposed site plan amendment would reduce by approximately 5% (1,845 s.f.) the amount of ground floor retail space, increase the number of residential units by 13 up to 154 units, and add balconies to an additional 43 units. No change is proposed to either the building’s height or tapering approved in 2007. The building would remain consistent with the density allowed under the site’s “C-O-2.5” zoning.

In addition, the applicant has agreed to provide the County a \$12,000 one-time contribution to support bike sharing in the County. Specifically, the monies would be used to support Arlington County’s bike sharing and bicycling initiatives within, or between Court House and Rosslyn.

**The following provides additional information about the site and location:**

- Site: The 73,562 square foot (1.689 acres) site is located on the south side of Wilson Boulevard (and north of Clarendon Boulevard) west of its intersection with North Troy Street. This site is within the Courthouse Metro Station area and within the boundaries of the Rosslyn to Courthouse Urban Design Study. The site has frontage on Wilson and Clarendon Boulevards, and all of the buildings that used to be on the site have been demolished. Uses adjacent to the site include the following:

- To the north: Wilson Boulevard and Colonial Village (residential) designated “Low-Medium” Residential on the General Land Use Plan (16-36 units per acre) with a note (Note #5) stating that that area was designated a “Coordinated Preservation and Development District” on 4/23/77. The site is zoned “RA7-16” Apartment Dwelling Districts.
- To the west: Wachovia Bank designated “General Commercial” on the General Land Use Plan. The property is zoned “C-2” Service Commercial-Community Business Districts. Wendy’s Restaurant is located further west and has the same General Land Use Plan designation and zoning (also some “C-3” zoning).
- To the east: An office building, which is designated “Service Commercial” on the General Land Use Plan and zoned “C-2” Service Commercial-Community Business Districts.
- To the south: Clarendon Boulevard. Courthouse Tower and small office building which are designated “High” Office-Apartment-Hotel on the General Land Use Plan and zoned “C-O” Commercial Office Building, Hotel and Multiple-Family Dwelling Districts. The Odyssey (condos) which are designated both “High” Office-Apartment-Hotel and “High-Medium” Residential (3.24 FAR Residential) on the General Land Use Plan and designated a “Special Affordable Housing Protection District” and zoned “RA-4.8” Apartment Dwelling Districts and “C-O” Commercial Office Building, Hotel and Multiple-Family Dwelling Districts.

Zoning: “C-O-2.5” Commercial Office Building, Hotel and Apartments Districts.

Land Use: The site is designated on the General Land Use Plan as “Medium” Office-Apartment-Hotel (Office up to 2.5 FAR; Apartment up to 115 units/acre; and Hotel up to 180 units/acre).

Neighborhood: The site is located within the Radnor-Fort Myer Heights Civic Association. The Colonial Village residential complex is located across Wilson Boulevard from the site. Colonial Village representatives and the Radnor-Fort Myer Heights Civic Association have been contacted and provided information about the proposed site plan amendment. None of the contacts have expressed to staff any concerns or issues regarding the proposed changes.

**Proposed Development:** The following table sets forth a statistical summary for the proposed development:

	Approved Plan (Approved February 2007)	Current Proposal
Original Site Area (prior to Feb. 2007 approval)	73,562 sf (1.68875 acres)	73,562 sf (1.68875 acres)
<b>Post-Dedication Site Area (w/o Troy St. &amp; alley)</b>	<b>69,242 sf (1.59 acres)</b>	<b>69,242 sf (1.59 acres)</b>
Post-Dedicated Site Area Allocated for Retail	13,874 sf (0.318 acres)	13,136 sf (0.299 acres)
Post-Dedicated Site Area Available for Residential	55,368 sf (1.27 acres)	56,106 sf (1.29 acres)
Post-Dedicated Site Area Used for Residential	53,408.4 s.f.(1.226 acres)	56,106 sf (1.29 acres)
Post-Dedicated site area left unused	1,959.6 s.f.	0 s.f.
Dedicated site area credit available for density (for Troy St. & alley)	4,320.0 s.f.	2,093 s.f.
<b>Leftover land area available for density (Condition #79)</b>	<b>6,279.6 s.f.</b>	<b>2,093 s.f.<sup>1</sup></b>
<b>Density</b>		
<b>Retail</b>		
Total GFA (FAR) over Post-Dedicated Site Area	34,685 sf (2.5 FAR)	32,840 sf (2.5 FAR)
<b>"C-O-2.5" Permitted Density - Commercial</b>	<b>2.5 FAR</b>	<b>2.5 FAR</b>
<b>Residential</b>		
<b>"C-O-2.5" Permitted Density - Residential</b>	<b>115 units per acre</b>	<b>115 units per acre</b>
<b>Building Height</b>		
Average Site Elevation	212.89 feet	212.89 feet
Main Roof Elevation	294.00 feet	294.00 feet
Main Roof Height	81.11 feet	81.11 feet
Penthouse Elevation	308.00 feet	308.00 feet
Penthouse Height	14.0 feet	14.0 feet
Number of Stories	6/7 stories	6/7 stories
<b>"C-O-2.5" Permitted Height</b>	<b>16 stories</b>	<b>16 stories</b>
<b>Parking</b>		
<b>Residential</b>		
Number of Spaces	162 spaces + 45 tandem spaces	176 spaces + 44 tandem spaces
Parking Ratio	1.14 spaces per unit (1.46 spaces per unit with tandem)	1.14 spaces per unit (1.42 spaces per unit with tandem)
<b>Required Residential Space (typ. site plan)</b>	<b>141 spaces</b>	<b>154 spaces</b>
<b>Required Residential Ratio (typ. site plan)</b>	<b>1 space per unit</b>	<b>1 space per unit</b>
<b>Retail<sup>2</sup></b>		
Number of Spaces	52 spaces	48spaces
Parking Ratio	1 space per 580 sf	1 space per 580 sf

<sup>1</sup> With the proposed site plan amendment, the applicant needed to use the equivalent of 4,186.6 s.f. of site area (1,959.6 s.f. of Post-Dedicated site area plus 2,227 s.f. of the Dedicated site area credit available for density (for Troy St. & alley)) to accommodate the proposed density, thus leaving 2,093 s.f. of site area to be used for density

<sup>2</sup> Per the Zoning Ordinance, the first 5,000 square feet of retail space in site plan within 1,000 feet of a metro station are exempt from parking requirements.

	Approved Plan (Approved February 2007)	Current Proposal
Required Retail Spaces (typ. site plan)	52 spaces	48 spaces
Required Retail Ratio (typ. site plan)	1 space per 580 sf	1 space per 580 sf
Visitor		
Number of Spaces	29 spaces + 9 tandems	32 spaces + 6 tandems
Handicap Spaces	11 spaces	11 spaces
Total Parking (including tandem)	243 spaces (297 with tandem spaces)	253 spaces (306 with tandem spaces)
<b>Coverage</b>	<b>56,275 sf (76.5%)</b>	<b>56,275 sf (76.5%)</b>
<b>LEED Score</b>	<b>26 points (Minimum for LEED Certification)</b>	<b>26 points (Minimum for LEED Certification)</b>

**DISCUSSION:** The applicant has proposed changing the ratio of commercial / residential for the project and has asked to modify the façade by adding more balconies. The applicant has agreed to provide the County with a one-time contribution of \$12,000 which would be used to support a bike sharing initiative along the Rosslyn-Ballston corridor in an area stretching from Rosslyn to Courthouse.

Density and Uses: After reviewing the residential and retail market, the applicant is proposing to increase by 13 the number of residential units in the building from 141 to 154 units. The unit mix was adjusted to provide a greater variety of unit types, e.g., studio and three bedroom units were added. This new unit mix is designed to help accelerate absorption of the units. The proposed changes are consistent with allowed densities under the site’s “C-O-2.5” zoning.

The approved plan did not use all of the site’s available density; Condition #79 noted the amount of available unused density, which was equivalent to 6,279.6 s.f. of land area. With the proposed site plan amendment, the applicant needed to use the equivalent of 4,186.6 s.f. of site area to accommodate the proposed density, thus leaving 2,093 s.f. of site area to be used for density; the use of such would necessitate a site plan amendment. Condition #79 has been modified to reflect the new amount of available site area that could be used for density purposes.

The approved plan included a multi-level underground garage (2.5 levels) containing 243 parking spaces (297 spaces with tandem parking), including 29 spaces specifically designated for visitor use outside of typical parking requirements for residential and retail uses. The proposed plan increases the number of parking to 253 parking spaces (306 spaces with tandem parking), including 32 spaces specifically designated for visitor use outside of typical parking requirements for residential and retail uses.

Retail entrances will front on both Wilson and Clarendon Boulevards. While the total amount of retail space is proposed to decrease by approximately 1,845 s.f., the amount of continuous linear street frontage of retail will not change from the approved plan. The project provides a large amount of retail space, which, when completed and fully leased, will significantly enhance both Clarendon and Wilson Boulevards.

Site and Design: The building fronts on both Wilson and Clarendon Boulevards. Entrances to both the residential and retail elements are provided on both street frontages. A driveway for the

garage and loading areas is located on the new (Troy) street (as identified in the Rosslyn to Courthouse Urban Design Study) on the east edge of the site. The curb cut is approximately 35 feet long.

The building will be constructed with facades of brick with precast coping lines and painted trellis systems incorporated into the corner balconies. Windows will be defined with a precast concrete header and brick sill and a painted aluminum mullion system. The building will be generally five stories above a ground floor level, incorporating tapered elements toward Wilson Boulevard and the eastern property line.

In the 2007 approval there were 43 residential units with balconies. In the current proposal, an additional 64 units gain a balcony, creating a total of 107 units with balconies. All of the balconies are within the property limits and have a similar design and materials to the balconies approved in 2007.

When the project was approved in 2007, it provided 52 retail parking spaces resulting in a parking ratio of 1 space per 580 s.f. – already taking into account the 5,000 s.f. GFA exclusion for the project's proximity to a Metro station. The applicant is proposing to decrease the amount of retail space by 1,845 s.f. but is keeping the retail parking ratio at the as-approved 1 space per 580 s.f.

**Site Plan Conditions:** To offset the impacts of 13 additional residential units in the project, the applicant has agreed to enhance their transportation demand management (TDM) plan to support Arlington County's bike sharing efforts. The applicant has agreed to make a one time contribution of \$12,000 to the program (Condition #51.r). In addition, Condition #51.e was updated to provide clarification as to whom (Arlington County Department of Environmental Services) and by when (prior to the issuance of the First Certificate of Occupancy) the developer must submit a \$5,000 contribution for bus stop improvements. Bike sharing helps to increase the mode choice options available to residents, employees and shoppers within Arlington, by making bicycles available to the public. Bike sharing is typically used to make short (1 to 3 mile) one-way trips that people would normally otherwise make by driving. With the combination of Arlington's already comprehensive bicycle network, a blossoming bicycle culture, and a publicly accessible bike sharing program, biking will increase its mode share, reducing the proportion of single occupancy vehicle trips made within the County. Arlington County is currently working with the District of Columbia to implement the first phase of its sharing program, which should be rolled out in the spring or summer of 2010.

In addition, the proposed project must use some of its available site area for density purposes. Thus, Condition #79 has been modified to reflect that the new amount of site area available to the applicant for density purposes has decreased to 2,093 s.f. The only other changes to site plan conditions (#1, 19, 64, 68, 77, and 78) are to reflect the new plans proposed by the applicant.

**CONCLUSION:** Staff supports the proposed site plan amendment. The changes will not impact the building's overall footprint/envelope and should make the project more likely to obtain financing and to ultimately get built. The additional balconies blend in with the rest of the architecture. The applicant's support of the County's bike sharing program will help promote

multi-modal transportation in the area. Therefore, staff recommends that the site plan amendment be approved, subject to all previous conditions of the hotel site plan, but with revisions to Conditions #1, 19, 51, 64, 68, 77 through 79.

1. **Site Plan Term**

The developer (as used in these conditions, the term “developer” shall mean the owner, the applicant and all successors and assigns) agrees to comply with the standard conditions set forth below and as referenced in Administrative Regulation 4.1 and the revised plans dated October 23, 2009, ~~January 18, 2007~~, and reviewed and approved by the County Board and made a part of the public record on December 12, 2009 ~~February 26, 2007~~, including all renderings, drawings, and presentation boards presented during public hearings, together with any modifications proposed by the developer and accepted by the County Board or vice versa.

This site plan approval expires three (3) years after the date of County Board approval if a building permit has not been issued for the first building to be constructed pursuant to the approved plan. Extension of this approval shall be at the sole discretion of the County Board. The owner agrees that this discretion shall include a review of this site plan and its conditions for their compliance with then current County policies for land use, zoning and special exception uses. Extension of the site plan is subject to, among other things, inclusion of amended or additional site plan conditions necessary to bring the plan into compliance with then current County policies and standards together with any modifications proposed by the owner and accepted by the County Board or vice versa.

**Pavement, Curb and Gutter Along All Frontages**

19. The developer agrees to show on the final engineering plans pavement, curb and gutter along all frontages of this site in accordance with the then-current Arlington County Standard for concrete curb and gutter and the then-current standards for pavement and according to the following dimensions. The pavement, curb and gutter shall be constructed prior to issuance of the first Certificate of Occupancy for occupancy of the applicable phase of the project.
- a. The developer agrees to construct new curb and gutter along Clarendon Boulevard, which shall be approximately 21.5 feet from the Arlington County survey centerline, but with variations, down to 15 feet from the survey centerline, due to an irregular curve in the road, as shown on the final engineering plan approved by the County Manager or his designee.
  - b. The developer agrees to construct new curb and gutter along Clarendon Boulevard, west of its site, for approximately 260 feet (measured from the western property line to the eastern curb of North Courthouse Road), approximately 21.5 feet from the Arlington County survey centerline, as shown on the final engineering plan approved by the County Manager or his designee.
  - c. The developer agrees to construct new curb and gutter along Wilson Boulevard approximately 21 feet from the Arlington County survey centerline, as shown on the final engineering plan approved by the County Manager or his designee.

- d. The developer agrees to construct a handicap ramp at the northwest corner and the southwest corner of the intersection of Clarendon Boulevard and the new North Troy Street and a crosswalk of materials as approved by the County, built per Arlington County Standards, across Clarendon Boulevard.
- e. The developer agrees to construct the interim configuration of North Troy Street, shown on “Plot and Location Plan—Interim” of the plans dated October 23, 2009, ~~January 18, 2007~~ (“North Troy Street (Interim)”). This includes, but is not limited to, curb and gutter on both sides, approximately 11 feet from the proposed centerline as shown on the final engineering plan approved by the County Manager or his designee. The dedication of this temporary easement is described in Condition #78.a.
- f. Within 120 days after notice by the County, which shall be effective only after the paving of the approximately 4,800 square foot portion of North Troy Street, located on the adjacent property to the east, the developer agrees to begin construction of all improvements for the portion of the ultimate configuration of North Troy Street that is located on the property, shown on the “Plot and Location Plan—Ultimate” of the plans dated October 23, 2009, ~~January 18, 2007~~ (“North Troy Street (Ultimate)”). This construction by the developer includes, but is not limited to, curb and gutter on both sides, approximately 11.5 feet from the proposed centerline as shown on the final engineering plan approved by the County Manager or his designee. The developer agrees to complete construction within 15 months from the date of notice by the County, as referenced above. The developer further agrees to dedicate the ultimate configuration of North Troy Street as described in Condition #78.b.

In addition, prior to the issuance of the first Certificate of Occupancy for the project, the developer agrees to submit to the Department of Environmental Services a performance bond estimate of the costs of construction of North Troy Street (Ultimate) and, upon approval of this estimate, a performance bond in a form approved by the County Manager as protecting the County’s interest, in the approved amount and in favor of the County, for the estimated cost construction and installation of all facilities within the ultimate configuration of North Troy Street (Ultimate) as described above. By way of illustration and not limitation, the bond shall provide that, if the developer does not commence or complete construction of the improvements for North Troy Street (Ultimate) within the above-stated time frame, the County has the authority to, upon demand, draw upon the bond in its full amount, to make such improvements. If the developer constructs the improvements, then this bond amount will be released to the developer upon completion of construction by the developer.

- g. The developer agrees to contribute \$125,000 to the County for the design, signalization and other such costs for signalization improvements installed at the intersection of Clarendon Boulevard and North Troy Street prior to the issuance of the final Certificate of Occupancy.

Unless the County Manager determines such compliance is prohibited by existing street grades, all improvements to curb, gutter, sidewalks and streets for pedestrian and/or vehicular access or circulation shall be in full compliance with the Americans with Disabilities Act (ADA) and any regulations adopted thereunder, as well as any other applicable laws and regulations. The developer further agrees that all improvements to curb, gutter, sidewalks, crosswalks, and streets for pedestrian and/or vehicular access or circulation shall be as determined by the County Manager on the final Site Development and Landscape Plan and on the final Site Engineering Plan, in accordance with the Rosslyn-Ballston Corridor Streetscape Standards or other applicable urban design standards in effect at the time of final Site Engineering Plan Approval; provided, however, that the provision of such improvements shall not increase the projected cost anticipated for such improvements as shown on the site plan drawings dated October 23, 2009, ~~January 18, 2007~~, unless the County provides additional funding to offset such increased cost.

#### **Transportation Management Plan**

51. The developer agrees to develop and implement a Transportation Management Plan (TMP) in order to achieve the desired results of the Arlington County Transportation Demand Management (TDM) program. The developer agrees to obtain the approval of the County Manager or his designee for such plan before the issuance of the first Certificate of Occupancy for the building.

Annual assessments rates will be adjusted for inflation by the U.S. Department of Labor, Bureau of Labor Statistics Consumer Price Index (CPI) Inflation Calculator from the date of site plan approval.

The Transportation Management Plan shall include a schedule and details of implementation and continued operation of the elements in the plan. The Transportation Management Plan shall include, but not be limited to, the following strategies:

#### **Participation and Funding**

- a. Maintain an active, on going relationship with Arlington Transportation Partners (ATP), or successor entity, at no cost to the developer, on behalf of the property management company.
- b. Designate a member(s) of building management as Property Transportation Coordinator (PTC) to be a primary point of contact with the county and undertake the responsibility for coordinating and completing all TMP obligations. The applicant and/or the building management will provide, and keep current, the name and contact information of the PTC to the Commuter Assistance Program. The Property Transportation Coordinator shall be appropriately trained, to the satisfaction of ACCS (Arlington County Commuter Services), to provide rideshare, transit, and other information provided by Arlington County intended to assist with transportation to and from the site.

- c. In addition to supporting the ongoing activities of the Property Transportation Coordinator and other commitments of this TMP, contribute \$5,000 per year for a period of ten (10) years to the Arlington County Commuter Services (ACCS) to sustain direct and indirect on-site and off-site services in support of TMP activities. Payment on this commitment will begin as a condition of issuance of the first Certificate of Occupancy for the first finished dwelling unit in the completed building. Subsequent payments will be made each year on the anniversary of the issuance of the first certificate of occupancy.

**Facilities and Improvements**

- d. Provide a Transportation Information Center Display (kiosk) purchased from or approved by ATP/ACCS, providing printed materials related to local transportation alternatives and maintain a stock of said materials at all times, in the residential building lobby.
- e. Prior to the issuance of the First Certificate of Occupancy the developer agrees to provide to the Arlington County Department of Environmental Services a one time contribution of \$5,000 for bus stop improvements within a ½ mile radius of the site yet to be determined.
- f. Maintain at least one on-site business center (including, at a minimum, access to copier, fax, pc, and internet services in a minimum 56 sq. ft. of space), which shall be made available to support residents of the building who choose to work from home.
- g. Comply with requirements of Site Plan conditions to provide bicycle parking/storage facilities, van access to the garage a parking management plan, and construction worker parking.

**Parking Management Plan**

- h. Comply with requirement of Site Plan conditions to develop a parking management plan that includes a description of employee, resident, visitor, and retail customer parking arrangements and charges; car sharing spaces, management of parking; and location of on-site parking meters and/or collection booths.
- i. Provide effective directional signage subject to approval of a Comprehensive Sign Plan (parking, deliveries, taxi stand, Zipcar, Flexcar, etc.) to direct residents and visitors to appropriate locations on the property.
- j. Depict, as part of the parking management plan, an area parking plan encompassing all block faces around the site. This plan will include a schematic drawing that designates proposed locations for, bus stops, loading zones for delivery vehicles, visitor bicycle rack locations, car sharing spaces, and on-street parking spaces. Additionally, this plan will note any restrictions as to times that various activities

(such as deliveries and parking) are permitted in the respective spaces.

- k. No on-street loading will be permitted between the hours of 7 and 9 a.m. and 4 to 6 pm.

### **Promotions, Services, and Policies**

- l. Provide preloaded SmarTrip cards to first time residents and employees, with a maximum value not to exceed \$10,000.
  - 1. **Residents** - Provide SmarTrip cards at a maximum cost to the developer of \$5.00 per card, plus \$20.00 Metro fare media per person, for free to the first residential lessee or first condominium purchaser of each unit, distributed no later than the day of move in at the building.
  - 2. **Property Management Employees** - Provide SmarTrip cards, at a maximum cost to the developer of \$5.00 per card, plus \$60.00 Metro fare media per person, for free to each on-site employee of the property management company and/or building operator at the time of the issuance of the first Certificate of Occupancy allowing residential occupancy. Provide or administer a sustainable commute benefit program for these employees (the program shall include, at a minimum, at the developer's option, pre-tax employee contributions and/or tax-free transit or vanpool monthly contributions.)
  - 3. **Retail and Commercial Employees** - Provide SmarTrip cards, at a maximum cost to the developer of \$5.00 per card, plus \$60.00 Metro fare media per person, for free to each initial on-site employee of each of the retail tenants at the initial lease-up of each retail space. The card and fare media shall be issued no later than the first day of work of the employee.
- m. Provide website hotlinks to CommuterPage.com<sup>TM</sup> under a "transportation information" heading from the developer and property manager's websites regarding this development.
- n. Distribute transit and ridesharing information to residents, visitors, and employees (and extend requirement to on-site businesses) to include the following items:
  - 1. Distribute in a new-resident package, material provided by Arlington County that includes site-specific transit-related information and SmarTrip cards to each new resident. Packages will be distributed to each tenant / owner no later than the day of their move-in at the building.
  - 2. Distribute in a new-employees package, materials provided by Arlington County including site-specific transit-related information and SmarTrip cards to all employees in the building. Packages will be distributed to each of the tenants' employees no later than their first full day of work at the building.

3. Distribute information provided by Arlington County regarding commute options and assistance services available.  
Cooperate with Arlington County to assist the County in implementing a transit-advertising program that will distribute information four times per year to all residents, tenants, employees, and visitors.
  4. Participate in Ozone Action Days and other regionally sponsored clean air, transit, and traffic mitigation promotions by posting notice of such promotions in locations within the building acceptable to the developer.
  5. Place a reference to the Courthouse Metro Station in promotional materials and advertisements.
- o. Provide a membership fee subsidy in a car sharing plan of thirty-five (\$35) per residential unit up to a limit of \$3000 for the site. This subsidy shall be paid on proof of membership in a car share service by first time or subsequent lessees and/or condominium purchasers.
  - p. Applicant agrees to implement the TMP.
  - q. Conduct transportation performance monitoring study at two and five years after issuance of first Certificate of Occupancy and provide a report summarizing findings, report findings to the County. Data collection for the site shall include a 3-day vehicle count at the garage entrance, counting both vehicles entering and exiting the garage. The data should be summarized to provided hourly vehicular counts, average daily traffic and AM, PM Midday peak hours for each day counted shall be identified, The study shall also include a voluntary mode split survey of residents and employees.
  - r. Prior to the issuance of the First Certificate of Occupancy the developer agrees to provide to the Arlington County Department of Transportation a one-time contribution of \$12,000 to support Arlington County's bike sharing program. Upon a request by the developer, the contribution, if not obligated by the County to the bike sharing program within five (5) years from the date of the Master Certificate of Occupancy, will be refunded to the developer.

#### **64. Retail Elements**

- a. The developer agrees to develop and implement a retail attraction and marketing plan for the 34,685 square feet of retail space located on the first floors of the residential buildings. Before implementation of the plan, the developer agrees to obtain the County Manager's review and approval of the plan as consistent with this site plan approval. The plan shall identify the types of retail desired, the marketing strategy to attract the retail, and strategies to retain the retail. The retail attraction and marketing plan shall be in accordance with the approved Retail Action Plan for the Rosslyn-Ballston Corridor, dated January 2001. The retail

attraction and marketing plan shall be reviewed and approved by the Department of Economic Development before being submitted to the Zoning Administrator. The above-grade building permit shall not be issued until documentation has been provided to the Zoning Administrator assuring that the plan has been approved by the Department of Economic Development. Any change in the use of the retail space from retail to office or other non-retail use shall require a site plan amendment.

- b. The retail spaces shall be designed and constructed to include interior and exterior improvements necessary to ensure that they are functional and attractive to prospective retailers and that they animate the street frontage. These elements shall include, but are not limited to: approximately 12-foot (minimum) floor to floor heights, as shown on the plans dated October 23, 2009, ~~January 18, 2007~~, access to the service corridor/areas as shown on the architectural plans dated October 23, 2009, ~~January 18, 2007~~, direct street frontage and access; rough-in of utilities, i.e., sprinkler heads, plumbing, electrical wiring, and stubs for extensions; provision for any venting systems required for any food preparation or restaurant use; and sufficient transparency of the building facade to achieve adequate street exposure.

#### **Building Security Requirements**

68.

- a. The developer agrees to coordinate with County staff on the design of exterior building security measures in order to limit or mitigate any adverse impacts that these measures may have on the project's urban design (including street and retail base) and streetscape. All exterior building security measures shall be shown on, and approved as part of, the final site development and landscape plan and the approved façade treatment plan. The base of the buildings, as shown in the drawings dated October 23, 2009, ~~January 18, 2007~~, and consistent with Condition #64 above, have been designed to accommodate retail uses and provide interest and activate the streetscape. Any change in the use and design of the base resulting from any proposal for exterior building measures shall require a site plan amendment.
- b. The developer agrees that it is the policy of the County to maintain the maximum number of on-street parking spaces around the perimeter of a site, and that it will not remove or reduce the number of on-street parking spaces around the perimeter of a site whether at the request of the developer or a tenant or otherwise. Accordingly, the developer agrees that it shall notify tenants of the aforesaid policy prior to execution of any lease with a tenant.

#### **Dedication of Alley Right-of-Way**

77. The developer agrees to dedicate, in fee simple, the land for the service alley along the western property line to the County Board of Arlington County prior to the issuance of the first Certificate of Occupancy for the building. This dedication shall include all land

(approximately 2,060 square feet) measured from the western property line to one (1) foot behind the proposed sidewalk, as shown on the plans dated October 23, 2009, ~~January 18, 2007~~, and shall be as a landscaped pedestrian access until the property immediately adjacent to the west redevelops, at which time, the developer of the property immediately adjacent to the west shall construct the service alley.

**Dedication of North Troy Street (Interim & Ultimate)**

78. The developer agrees to a phased dedication of a new portion of North Troy Street to the County, as described below.

- a. The first phase of this phased dedication requires the developer to file a plat dedicating an approximately 3,900 square foot temporary public use access easement to the County Board of Arlington County, as shown on the “Plot and Location Plan—Interim” dated October 23, 2009, ~~January 18, 2007~~, prior to issuance of the first Certificate of Occupancy allowing tenant occupancy.

Therefore, to effectuate the above stated first phase of dedication, the developer hereby agrees to grant a temporary public use access easement to the County Board of Arlington County for the benefit of the County and the public at large, for access to and use, at all times, except as necessary for street maintenance and repairs, by the County and the public at large, of the street, and sidewalks located on and adjacent to Part Lot 16 and Lots 25-A and 25-B of the Subdivision of Washington View, RPC #'s 17011016 and 17011009, as shown on (“Plot and Location Plan – Interim”) prior to the issuance of the first Certificate of Occupancy allowing tenant occupancy. The developer agrees that the County shall have the right to use and control the streets to the same degree as any other streets in the County system. The public access easement shall not obligate or require the County to construct or maintain the street or sidewalks (which shall be the developer’s responsibility pursuant to Condition #19 and shall include snow removal Condition #60), but shall permit public use of the easement areas for street, sidewalk, utilities (to the extent such utilities are not inconsistent with other utilities required by this site plan as approved in the final site engineering plan), and related purposes and shall permit the County to regulate traffic and parking. The public access easement shall provide, among other things, that the grantor, its successors and assigns, shall indemnify and hold harmless the County Board, its elected and appointed officials, employees and agents from any liability, claims, damages, costs and expenses of whatever nature concerning or arising out of the construction, maintenance, and regulation of the private street by the developer and use thereof by the general public and the developer or grantor except to the extent said liability, claims, damages, costs and expenses are caused solely by the negligence of the County. The public access easement shall be granted by deed, in form and substance acceptable to the County Manager and the County Attorney, shall include the terms of the easement as set forth in this condition, and shall be recorded among the land records of the Clerk of the Circuit Court of Arlington County. Improvements necessary for the use of this public access easement by the public at large shall be completed prior to the issuance of the first

Certificate of Occupancy allowing tenant occupancy for any building adjacent to this public access easement. The developer shall be, and the public access easement shall state, that the developer will be responsible for maintaining this public access easement, and all adjoining and adjacent improvements located on developer's parcels. The deed dedicating this easement shall specify that the easement will automatically extinguish upon the recording of the subsequent fee simple dedication, as described below in Paragraph b of this condition.

- b. The second phase of this phased dedication shall occur upon the redevelopment of the parcels immediately adjacent to developer's parcels to the east. The developer agrees to construct the balance of the improvements shown on the "Plot and Location Plan —Ultimate," dated October 23, 2009, ~~January 18, 2007~~, as further described in Condition #19f. Such construction shall begin within 120 days after notice by the County, which shall be effective only after the paving of the approximately 4,800 square foot portion of North Troy Street, located on the adjacent property to the east. In addition, the developer agrees to dedicate, in fee simple, within 90 days from the completion of construction as called for in Condition #19.f., the approximate 2,260 square feet of land necessary for the creation and final alignment of North Troy Street, as shown on the "Plot and Location Plan—Ultimate," dated October 23, 2009, ~~January 18, 2007~~.
- c. In association with the creation and dedication of this new portion of North Troy Street, the developer agrees that all new electrical transformers shall be placed underground in vaults which meet Virginia Power standards. These vaults may be placed in the initial temporary public access easement or the final new portion of North Troy Street, if approved by the County on the final site engineering plan. The ventilation grates shall be located in compliance with Condition #38.

**Availability of unused density**

- 79. The developer agrees that there is 2,093 s.f. of site area available ~~the remaining potential density~~ on the property included in SP #389, after road dedications, to be used as potential density. is 16 residential units or 26 hotel units or 15,699 square feet of commercial GFA. ~~The 2,093 s.f. of site area at current "C-O-2.5" district limits could yield up to 5 residential units or 8 hotel units or 5,232 s.f. of commercial GFA.~~ The developer further agrees that future use of this site area for density is subject to the approval of a future site plan amendment by the County Board.

PREVIOUS COUNTY BOARD ACTIONS:

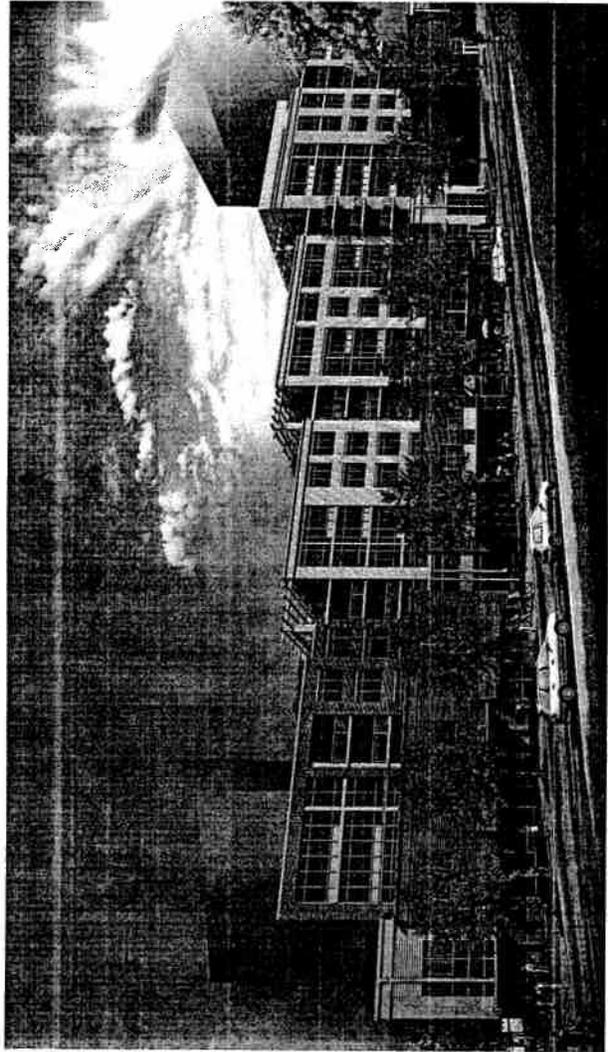
(SP #389 – 2001 Clarendon Blvd., aka, 2000 Wilson Blvd.)

September 17, 2005	Denied for 2000 Wilson Blvd. a General Land Use Plan amendment request from “General Commercial” to “Medium” Office-Apartment-Hotel; a Rezoning request (Z-2520-05-1) from “C-2” to “C-O-2.5,” and a site plan request (SP #389) for approximately 165 residential units, and 32,468 s.f. commercial/retail space in one building.
January 30, 2007	Deferred a GLUP Amendment, Rezoning, and site plan request to February 24, 2007.
February 26, 2007	Approved for 2000 Wilson Blvd. a General Land Use Plan amendment request from “General Commercial” to “Medium” Office-Apartment-Hotel; a Rezoning request (Z-2520-05-1) from “C-2” to “C-O-2.5,” and a site plan request (SP #389) for approximately 141 residential units, and 34,685 s.f. commercial/retail space in one building, and a comprehensive sign plan.

# 2001 Clarendon Blvd

ARLINGTON COUNTY, VA  
SKYLARK INVESTMENTS, LLC

**RECEIVED**  
OCT 27 2009  
By \_\_\_\_\_



## MINOR AMENDMENT October 23rd, 2009

**OWNER**  
SKYLARK INVESTMENTS, LLC  
C/O HLM STREET DEVELOPMENT  
1355 BEVERLY ROAD  
SUITE 240  
MCLEAN, VA 22101  
TEL: 703 734 9730  
CONTACT: JIM MOBLEY

**ARCHITECT**  
WDG ARCHITECTURE, PLLC  
1095 Connecticut Avenue NW  
Suite 300  
Washington, DC 20036  
TEL: 202 857 8300 CONTACT: SAL  
MIKEDACH

**LEED CONSULTANT**  
ABIGAIL S. CURRIE, LEED AP  
PAGE SOUTHERLAND PAGE, LLP  
3101 WILSON BLVD. SUITE 300  
ARLINGTON, VA 22201  
TEL: 703 527 4100

**CIVIL ENGINEER**  
VKA INCORPORATED  
8180 GREENSBORO DRIVE  
SUITE 200  
ALEXANDRIA, VA 22102  
TEL: 703 442 8000  
CONTACT: ED IGNACIO

**LANDSCAPE ARCHITECT**  
STUDIO 39  
6416 GROVEDALE DR.  
SUITE 100-A  
ALEXANDRIA, VA 22310  
TEL: 703 719 6500  
CONTACT: LOREN HELGASON

**TRAFFIC CONSULTANT**  
WELLS & ASSOCIATES, LLC  
5 WIRT STREET SW  
SUITE 300  
LEESBURG, VA 20175  
TEL: 703 443 1442  
CONTACT: CHRIS TURNBULL

**ATTORNEY & APPLICATION MADE BY**  
WALSH, COLUCCI, LUBBELEY, EMRICH &  
WALSH, P.C.  
2200 CLARENDON BLVD. 13TH FLOOR  
ARLINGTON, VA 22201  
TEL: 703 528 4700  
CONTACT: NAN WALSH

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fax 202 463 8198  
e-mail wdg@wdgarch.com

SKYLARK INVESTMENTS, LLC

2001 CLARENDON BOULEVARD  
ARLINGTON COUNTY, VIRGINIA  
REVISED: 10-23-09



Cover Sheet  
\$0.00  
No. 10000000  
A0.00

### Drawing List

#### Architecture Drawings

AO 00	COVER SHEET
AO 01	UNIT MIX
A1 00	AERIAL VIEW
A1 01	SITE PLAN ULTIMATE
A1 02	SITE PLAN INTERIM
A1 03	PARKING G-3 FLOOR PLAN
A1 04	PARKING G-2 FLOOR PLAN
A1 05A	G1A GROUND FLOOR PLAN
A1 05B	G1B GROUND FLOOR PLAN
A1 06	1ST FLOOR PLAN
A1 07	2ND FLOOR PLAN
A1 08A	3RD FLOOR PLAN
A1 08B	4TH FLOOR PLAN
A1 09	5TH FLOOR PLAN
A1 10	6TH FLOOR PLAN
A1 11	7TH FLOOR PLAN
A1 12A	PH FLOOR PLAN
A1 12B	ROOF PLAN
A2 01	BUILDING SECTIONS
A2 02	BUILDING SECTION
A3 01	SOUTH ELEVATION
A3 02	NORTH ELEVATION
A3 03	EAST ELEVATION
A3 04	WEST ELEVATION
A3 05	INTERIOR ELEVATIONS
A3 06	INTERIOR ELEVATIONS
A3 07	ENLARGE SOUTH ELEV.
A3 07A	ENLARGE SOUTH ELEV.
A3 08	ENLARGE NORTH ELEV.
A3 08A	ENLARGE NORTH ELEV.
A3 09	ENLARGE EAST ELEV.
A3 10	ENLARGE WEST ELEV.
A6 01	CANOPY DETAILS
A6 02	CANOPY DETAILS
A6 03	CANOPY DETAILS
A6 13	TRELLIS DETAILS

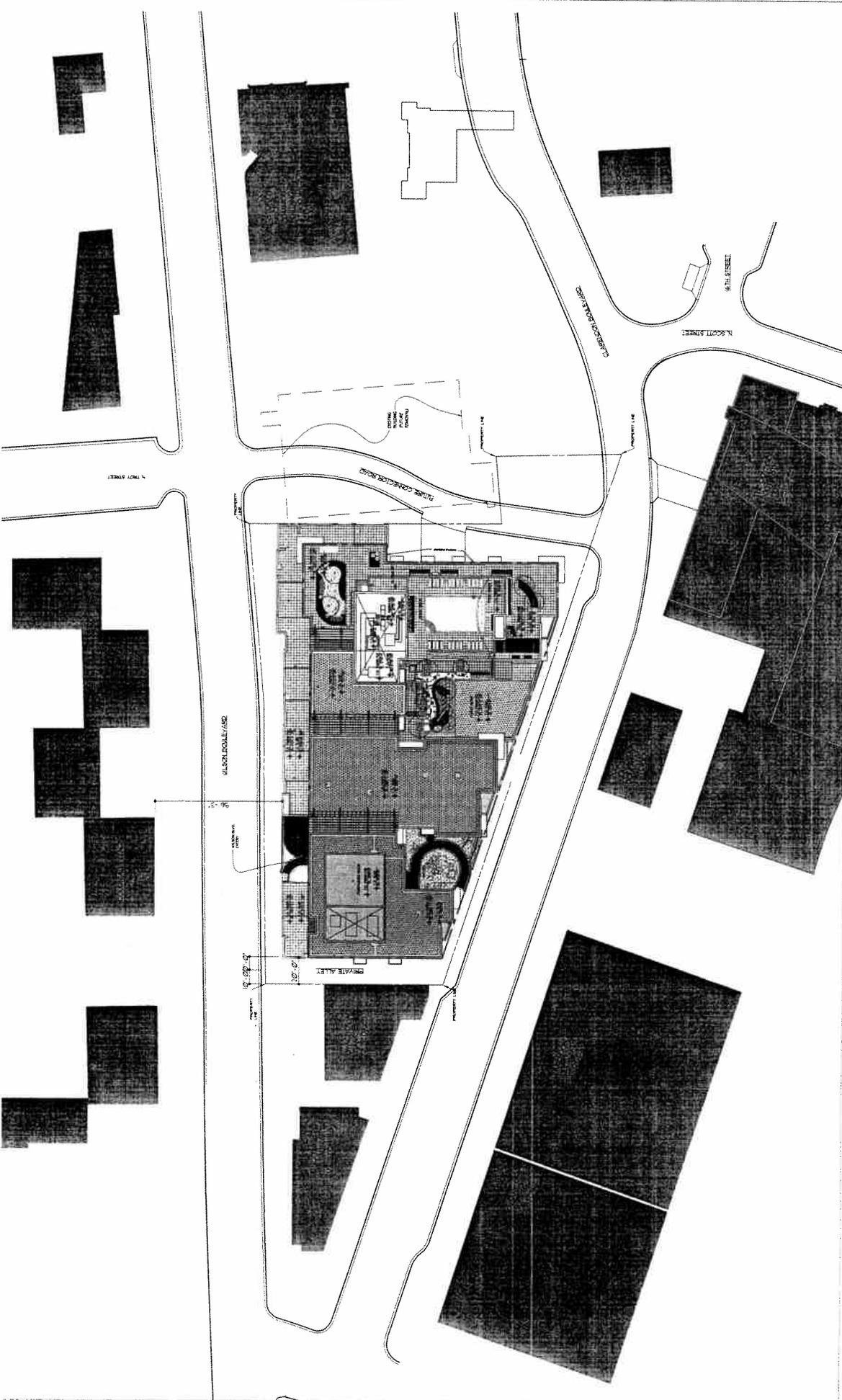
#### Civil

C-1	CERTIFIED SURVEY PLAT
C-2	CERTIFIED SURVEY PLAT
C-3	REZONING PLAT
C-4	PLOT & LOCATION PLAN INTERIM
C-5	PLOT & LOCATION PLAN ULTIMATE
C-6	PEDESTRIAN ENTRANCES
C-7	STRIPING PLAN INTERIM
C-8	CLARENDON BLVD. STREET SECTIONS
C-9	WILSON BLVD. STREET SECTIONS
C-10	TROY STREET SECTIONS
C-11	TRIP SECTION
C-12	STREET LIGHT MANAGEMENT COMPUTATIONS
C-13	CONTEXT PLAN

#### Landscaping

L1 1	EXISTING TREE SURVEY
L1 2	EXISTING TREE SURVEY
L1 3	TREE PRESERVATION CALCULATIONS
L2 1	OVERALL STREETScape PLAN
L2 2	OVERALL SITE PLAN
L2 3	WILSON ENTRANCE DETAILS
L2 3	PLANT LIST
L3 1	2000 WILSON BLVD. INTERIM TROY STREET SITE PLAN
L3 1	2000 WILSON BLVD. ULTIMATE TROY STREET SITE PLAN





Site Plan  
 Ultimate  
 06/20/07  
 WARD3  
 Major Amendment  
 10/22/09



2001 CLARENDON BOULEVARD  
 ARLINGTON COUNTY, VIRGINIA  
 REVISED 10-22-09

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 e-mail wdg@wdgarch.com

SKYLARK INVESTMENTS, LLC

96  
 100-01  
 PRIVATE ALLEY  
 ELITE CONNECTION BOULDER  
 N. SCOTT STREET  
 N. FIRST STREET  
 CLARENDON BOULEVARD

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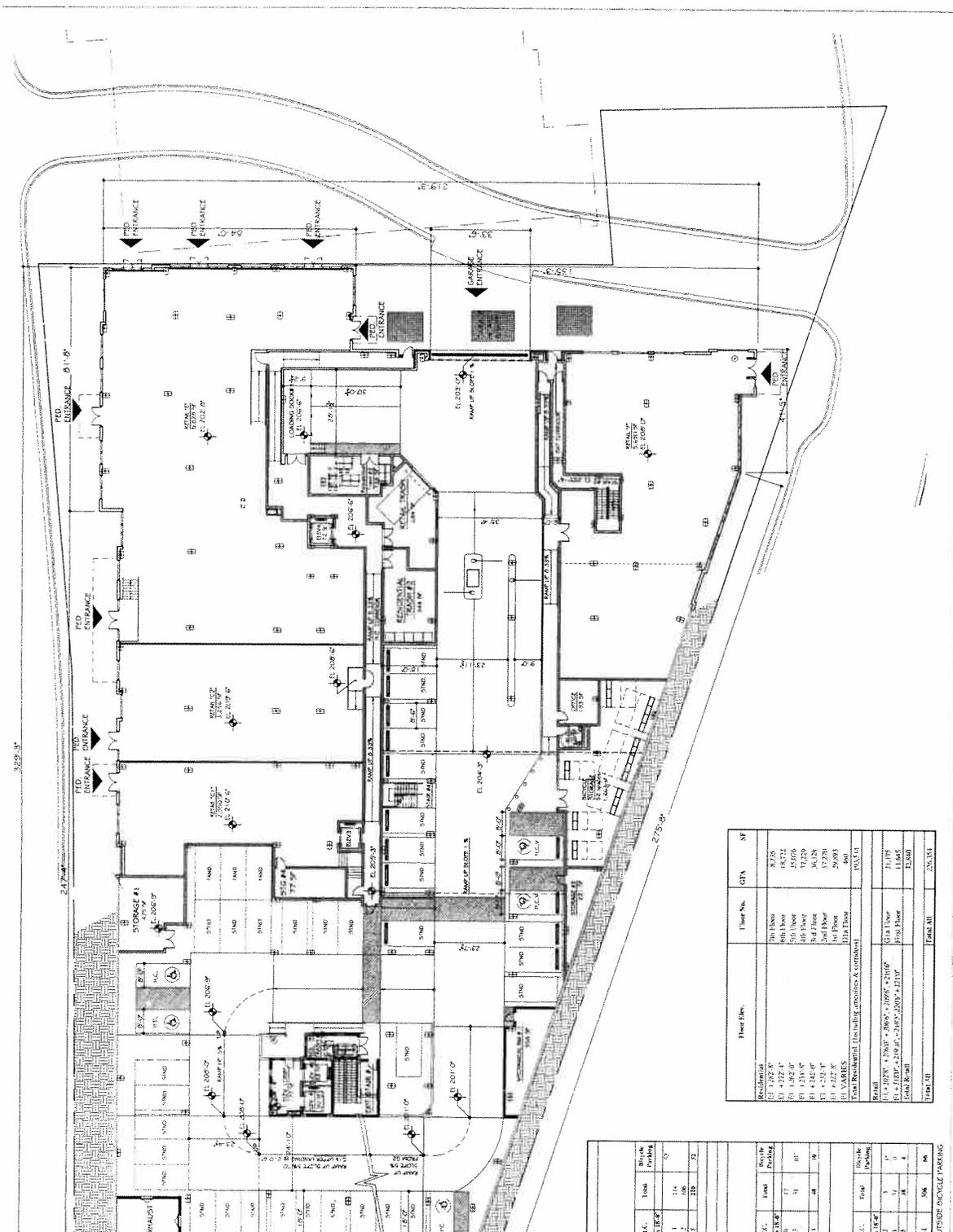
ELITE CONNECTION BOULDER

CLARENDON BOULEVARD

N. SCOTT STREET

N. FIRST STREET

PROPERTY LINE



Parking Requirements		Residential Parking		Retail Parking		Total Parking	
Floor No.	Area (sq ft)	Req'd	Provided	Req'd	Provided	Req'd	Provided
1st Floor	100,000	10	10	5	5	15	15
2nd Floor	100,000	10	10	5	5	15	15
3rd Floor	100,000	10	10	5	5	15	15
4th Floor	100,000	10	10	5	5	15	15
5th Floor	100,000	10	10	5	5	15	15
6th Floor	100,000	10	10	5	5	15	15
7th Floor	100,000	10	10	5	5	15	15
8th Floor	100,000	10	10	5	5	15	15
9th Floor	100,000	10	10	5	5	15	15
10th Floor	100,000	10	10	5	5	15	15
11th Floor	100,000	10	10	5	5	15	15
12th Floor	100,000	10	10	5	5	15	15
13th Floor	100,000	10	10	5	5	15	15
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18th Floor	100,000	10	10	5	5	15	15
19th Floor	100,000	10	10	5	5	15	15
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21st Floor	100,000	10	10	5	5	15	15
22nd Floor	100,000	10	10	5	5	15	15
23rd Floor	100,000	10	10	5	5	15	15
24th Floor	100,000	10	10	5	5	15	15
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41st Floor	100,000	10	10	5	5	15	15
42nd Floor	100,000	10	10	5	5	15	15
43rd Floor	100,000	10	10	5	5	15	15
44th Floor	100,000	10	10	5	5	15	15
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97th Floor	100,000	10	10	5	5	15	15
98th Floor	100,000	10	10	5	5	15	15
99th Floor	100,000	10	10	5	5	15	15
100th Floor	100,000	10	10	5	5	15	15

**WDG ARCHITECTURE**  
1095 Connecticut Avenue NW  
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Washington, DC 20036  
tel 202.463.8300  
fax 202.463.8398  
email wdg@wdgarch.com

**GIA Ground Floor Plan**  
10.17.07  
WA0013  
Major Revisions  
11.02.07

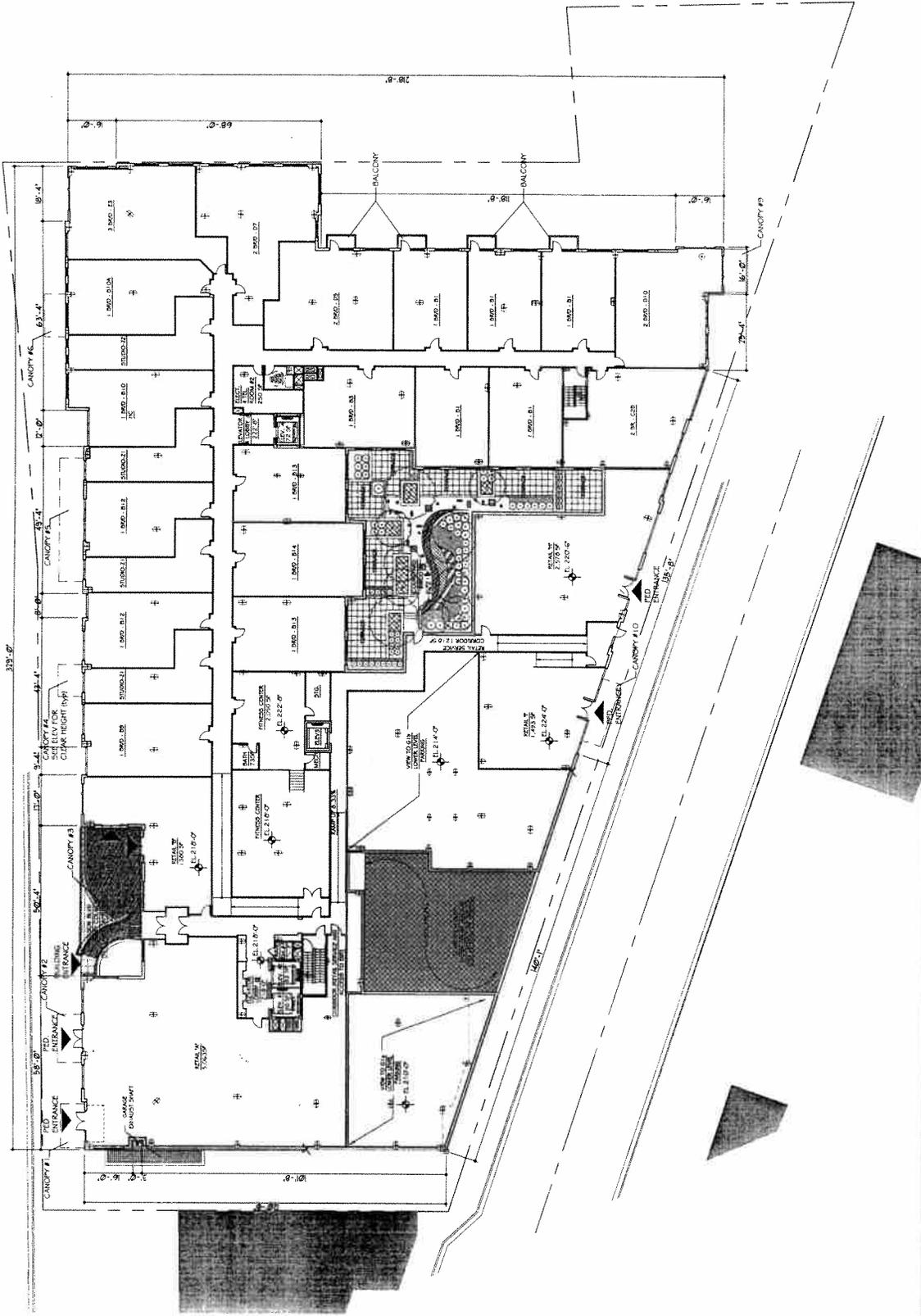
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2001 CLARENDON BOULEVARD  
ARLINGTON COUNTY, VIRGINIA  
REVISED: 10-23-09  
REVISED: 11-23-09

**WDG ARCHITECTURE, PLLC**  
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**SODARK INVESTMENTS, LLC**

NOTE: OUTSIDE BICYCLE PARKING



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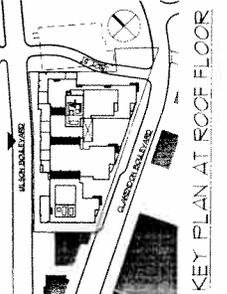
SKYLARK INVESTMENTS, LLC

2001 CLARENDON BOULEVARD  
 ARLINGTON COUNTY, VIRGINIA  
 REVISED: 10-28-09

First Floor Plan  
 04.07.07  
 WARD 3  
 New Amendment  
 2007-10-28-09

A1.06

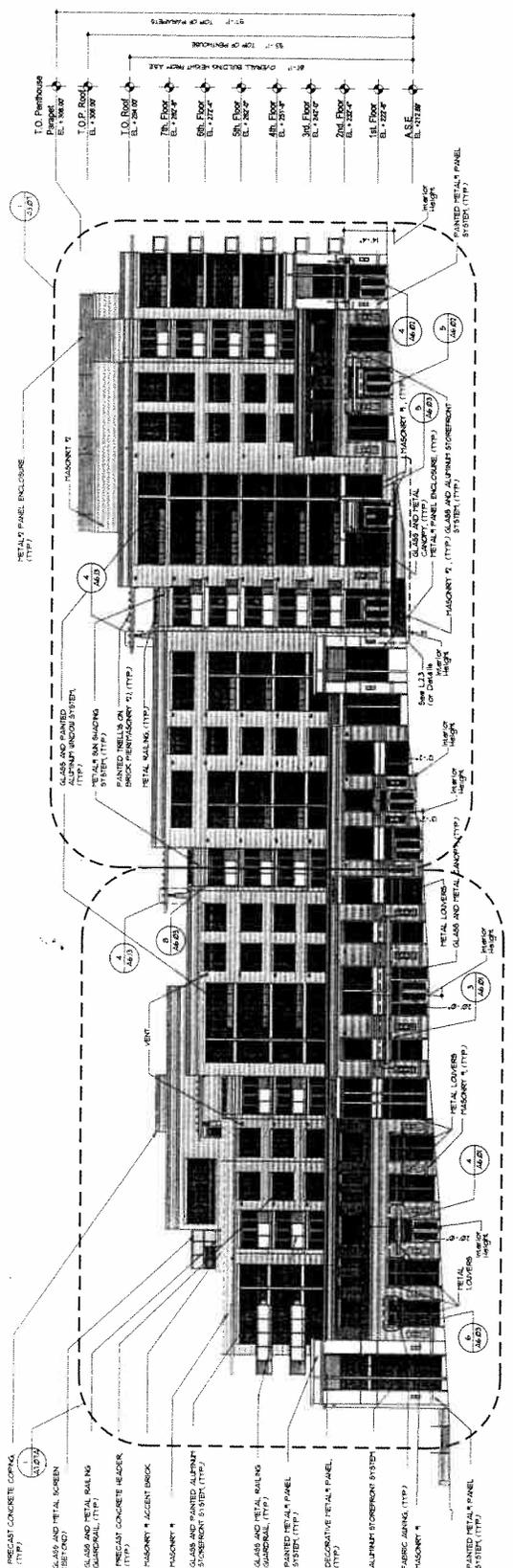




KEY PLAN AT ROOF FLOOR

MATERIAL SCHEDULE

MATERIAL	COLOR DESCRIPTION
MASONRY #1	REDDISH BROWN BRICK
MASONRY #2	BUFF BRICK
MASONRY #3	FACTORY FINISHED
MASONRY #4	BUFF



WILSON BLVD. ELEVATION

NOTE: DIMENSIONS AND SPACINGS ARE INDICATED IN FEET AND INCHES. ALL DIMENSIONS AND SPACINGS SHALL BE CONSISTENT WITH THE COMPREHENSIVE SIGN PLAN.

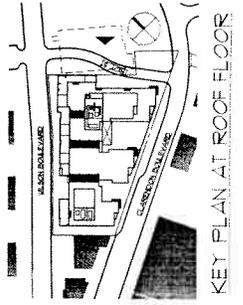
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 fax 202 463 8198  
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SKYLARK INVESTMENTS, LLC

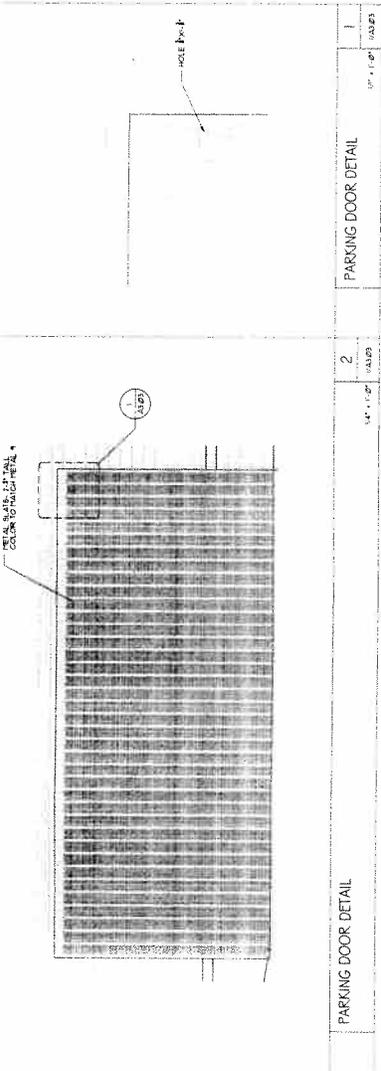
2001 CLARENDON BOULEVARD  
 ARLINGTON COUNTY, VIRGINIA  
 REVISED: 10-28-09

North Elevation  
 SUBJ: A3.01  
 Project Number: 10-28-09  
 Scale: 1/8"=1'-0"



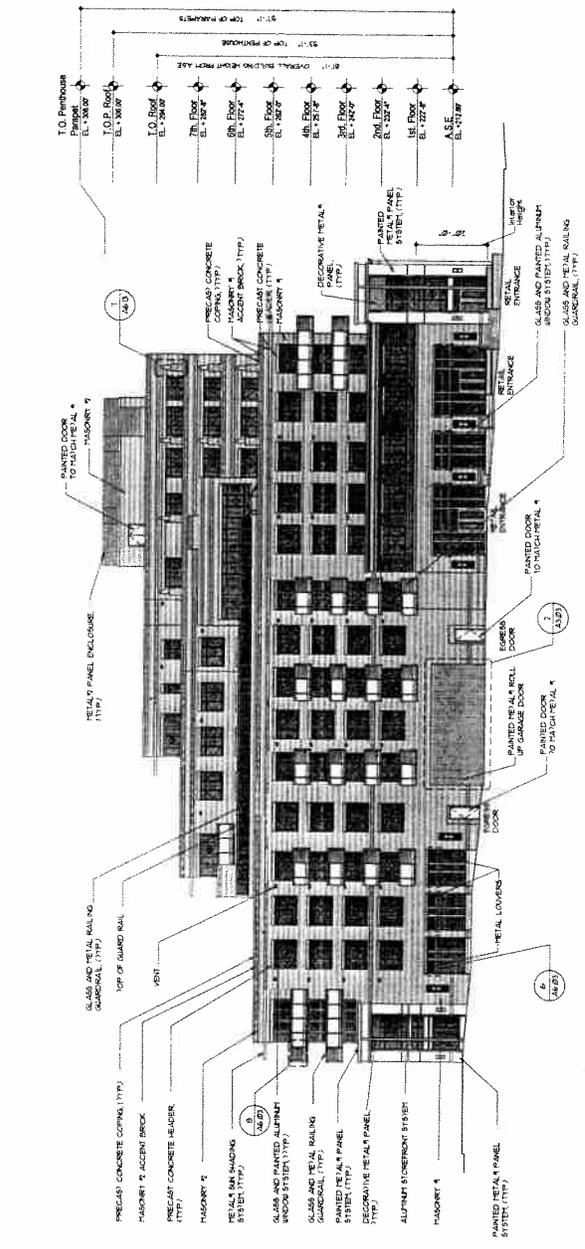


KEY PLAN AT ROOF FLOOR



PARKING DOOR DETAIL

MATERIAL	COLOR DESCRIPTION
MASONRY #1	REDDISH BROWN BRICK
MASONRY #2	BUFF BRICK
METAL #1	FACILITY FINISHED
PRECAST CONCRETE	ENF



EAST ELEVATION

NOTE: CANDLES, AWNINGS AND STOREFRONTS ARE SUBJECTIVE IN NATURE AND SUBJECT TO CHANGE CONSISTENT WITH THE COMPREHENSIVE SIGN PLAN

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Suite 300  
Washington, DC 20036  
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SKYLARK INVESTMENTS, LLC

2001 CLARENCE BOULEVARD  
ARLINGTON COUNTY, VIRGINIA  
REVISED: 10-25-09

East Elevation  
03.07.07  
W000013  
A3.03  
Scale: 1/8" = 1'-0"





**SP # 389**  
**2020 Wilson Blvd. and 2021 Clarendon Blvd**  
**RPC#s 17-011-009, -013, -015, -016**

Note: These maps are for property location assistance only.  
 They may not represent the latest survey and other information.



Not To Scale

